



FRIENDS OF THE  
DOVER GREENWAY

Dover Town Meeting

May 1, 2017



# The Dover Greenway Vision





# Protections for Dover Residents

- No request for Town funds (FDG will gift to Town)
- Limits usage volume by restricting scope of trail
- Prevents loss of land to other potential lessees
- Removes rotting, contaminated ties / caps rail bed
- Dover does not assume responsibility for historical environmental liability

*Did you know.....*

There are over 60 rail trails just in MA and an additional 68 projects in progress totaling 300 miles of rail trails.

[www.railstotrails.org/our-work/united-states/massachusetts/](http://www.railstotrails.org/our-work/united-states/massachusetts/)

# Environmental liability

- ✓ Possibility of Dover rail bed contamination requiring soil clearance is extremely low (rural trail, no visible or recorded contamination)
- ✓ Legal responsibility for residual soil contamination remains with prior owners (Mass law 21E)
- ✓ Responsibility for contamination that occurs during rail trail construction covered by environmental insurance policy
- ✓ Best Practice to address existing rail bed is remove the ties, cap the bed with stone dust (Mass DEP) = a rail trail

# Environmental Improvement

- **Licensed Site Professionals Letter**
  - Each LSP resides in Dover
  - “Environmental Plus”
- **MA Department of Environmental Protection**
  - Thomas Mahin and James Persky, Drinking Water regional contacts\*
  - Environmental conditions would improve with rail trail
- **Colonial Water**
  - Phone conversation with Alan Melancon, November 17, 2015\*
  - No history of water quality concerns





# A Trail for Everyone

- Respect all constituencies in trail design and planning
  - Low-maintenance, rural trail
  - Privacy screening
  - Retain horse trail crossings, protect with signage
  - Protects the hunt club tradition
- Safety, home values and crime data benefit all





# No Financial Burden on Tax Payers

## Projected cost

- Beals & Thomas estimate: \$1.2M including environmental insurance\*

## Maintenance of the trail

- \$50,000 maintenance escrow in estimate upfront (10 years projected expense)
- Friends of Dover Greenway will coordinate volunteer trail maintenance (60% of rail trails in US maintained by volunteers)



# If We Vote “NO” Tonight...

- Town of Dover misses opportunity to acquire this land;
  - Weston > DCR and Eversource acquired rail corridor
  - Sudbury > Eversource plan a 115-kilovolt (kV) transmission line
  - This has only occurred when towns did not lease their corridor
- Possibility of multi-town trail remains open (DCR, BCRTA)
- Missed opportunity to have a recreational resource accessible by all of Dover

*“55 percent of trail users exercising more now than before they had access to the trail”\**





# Benefits for all Dover Residents

- **Improve public safety, expand resources**
  - Safer in-town connections & recreation
  - Only ADA-accessible path allowing recreation for all
  - Environmental safeguard
- **Dover control of corridor**
  - Leverage town use
  - Economic value
- **Greater sense of community**
  - Connecting people and places



*Be Part of a Legacy for Dover*



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Thank you.

Please vote "YES" on Article 19